

# LONDON BOROUGH OF ENFIELD

## PLANNING COMMITTEE

**Date :** 12<sup>th</sup> September 2017

**Report of**  
Assistant Director,  
Regeneration & Planning

**Contact Officer:**  
Andy Higham  
Sharon Davidson  
Josleen Ray Tel: 020 8379 4981

**Ward:** Upper Edmonton

**Ref:** 17/02952/RM

**Category:** Reserved Matters (Major)

**LOCATION:** Meridian Water, Willoughby Lane and Meridian Way, London

**PROPOSAL:** Submission of reserved matters pursuant to condition 5, part (i) of outline planning permission ref: 16/01197/RE3 in respect of Layout in relation to the Station Building Site only for the new Station Building and platform (to replace Angel Road Station) including a pedestrian link across the railway at the Phase 1 site.

**Applicant Name & Address:**  
Network Rail Infrastructure Ltd.  
c/o Agent

**Agent Name & Address:**  
Daniel Chalk  
Network Rail Infrastructure Ltd.  
1 Eversholt Street  
London

### RECOMMENDATION:

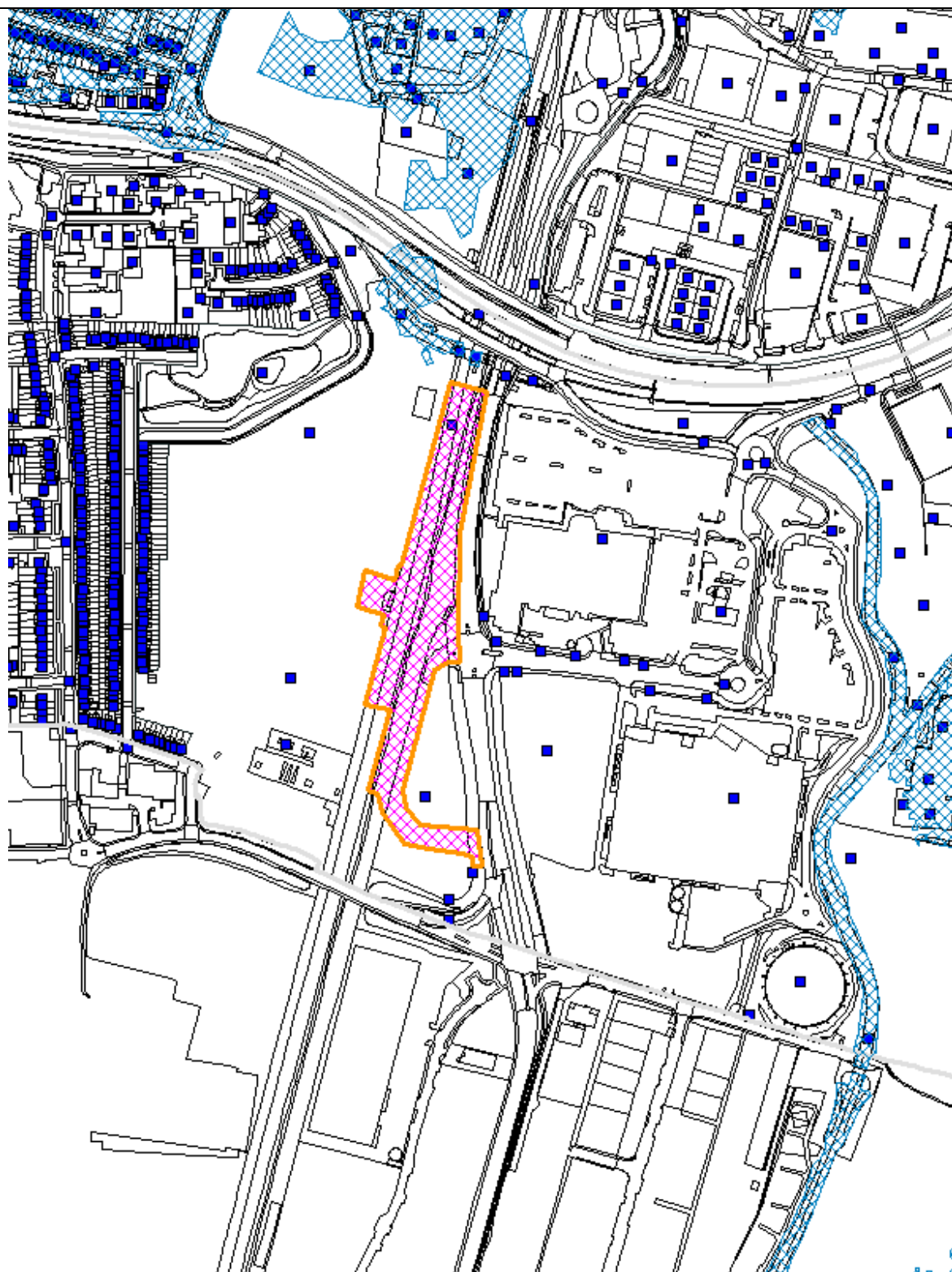
(i) Approve reserved matters required by condition 5, part (i) (Layout) in respect of the Meridian Water Station Building Site only, pursuant to Outline Planning Permission ref: 16/01197/RE3 dated 10/07/2017 subject to the conditions listed in section 9 and;

(ii) Agree to grant the Head of Development Management delegated authority to **APPROVE** subsequent reserved matters required by condition 5, parts (ii)-(iv) in relation to the Meridian Water Station Building Site pursuant to Outline Planning Permission ref: 16/01197/RE3 dated 10/07/2017 on the basis of the illustrative details presented in this report.

### Plan Numbers:

MERIDIAN-WATER-PLANNING-DRG-004 rev. A, 280-A-P-001-01 rev. 02, 280-A-P-001-02 rev. 01, 280-A-P-100-01 rev. 01, 280-A-P-100-10 rev. 01, 280-A-P-100-11 rev. 00.

Ref: 17/02952/RM LOCATION: Meridian Water, Willoughby Lane And, Meridian Way, London



## 1. Site and Surroundings

- 1.1 The application site relates to an area of land defined as the Station Building Site (Figure 1) which lies within the boundary of the Meridian Water Phase 1 Outline Planning Application Site towards its eastern boundary with Meridian Way. The Phase 1 Outline Site shown outlined in blue in Figure 1 received outline planning permission in July 2017 for comprehensive redevelopment as part of the phased redevelopment of the wider Meridian Water Project Area (Meridian Water Masterplan, 2013) which included the provision of a new Network Rail Train Station to replace the existing facility at Angel Road.

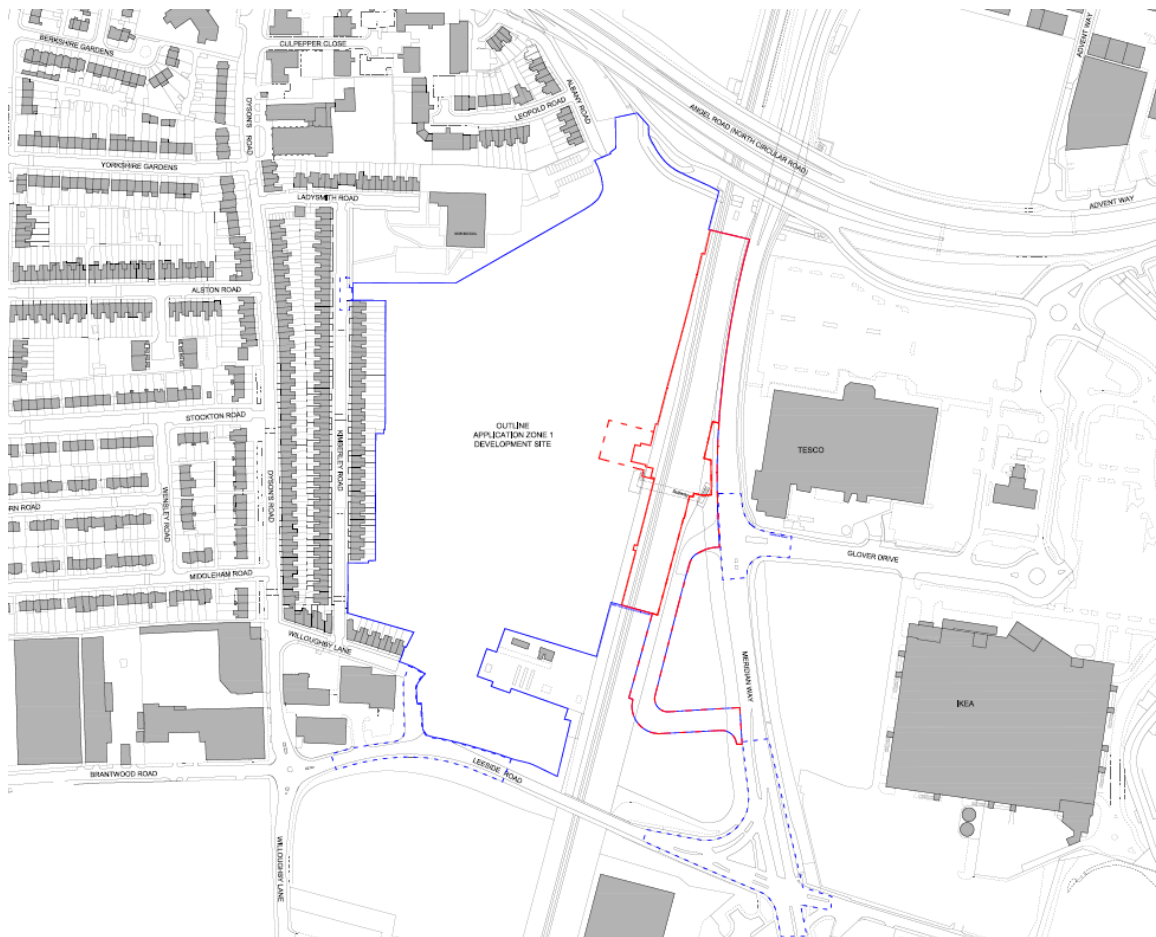


Figure 1 - Station Building Site, Phase 1 Outline Site.

- 1.2 The wider outline application site (Phase 1) is approximately 8ha and comprises the former gas holder site on Willoughby Lane on the west side of the railway line, part of the 'tear drop' site to the east of the railway lines and a small stretch of Pymmes Brook.
- 1.3 The site extends from the North Circular Road to the north, to Leeside Road to the south. It bounds Albany Road and the site of the proposed new Meridian Angel Primary School on Ladysmith Open Space to the North West. Residential

properties in Kimberley Road and Willoughby Lane bound the site to the west; Meridian Way forms the eastern boundary. The site wraps around an operational pressure reduction station (PRS), owned by National Grid Gas, and which for the present time will remain in situ and operational.

- 1.4 The site adjoins the Borough boundary with the London Borough of Haringey to the south.
- 1.5 The Station Building Site as defined by the solid red line in Figure 1 above lies to the east of the wider Phase 1 site and adjoins Meridian Way and future phases of the Meridian Water Project Area. It encompasses the existing railway lines and sidings which currently forms part of the West Anglia Mainline wildlife corridor.
- 1.6 Figure 2 below shows the illustrative relationship of the proposed station platforms and entrance with the buildings within Phase 1 which will be the subject of further reserved matters proposals pursuant to the Outline Planning Permission ref: 16/01197/RE3 (OPP1). The station is a key piece of new transport infrastructure that will also facilitate movement east/ west through Meridian Water through the provision of a bridge across the railway lines.

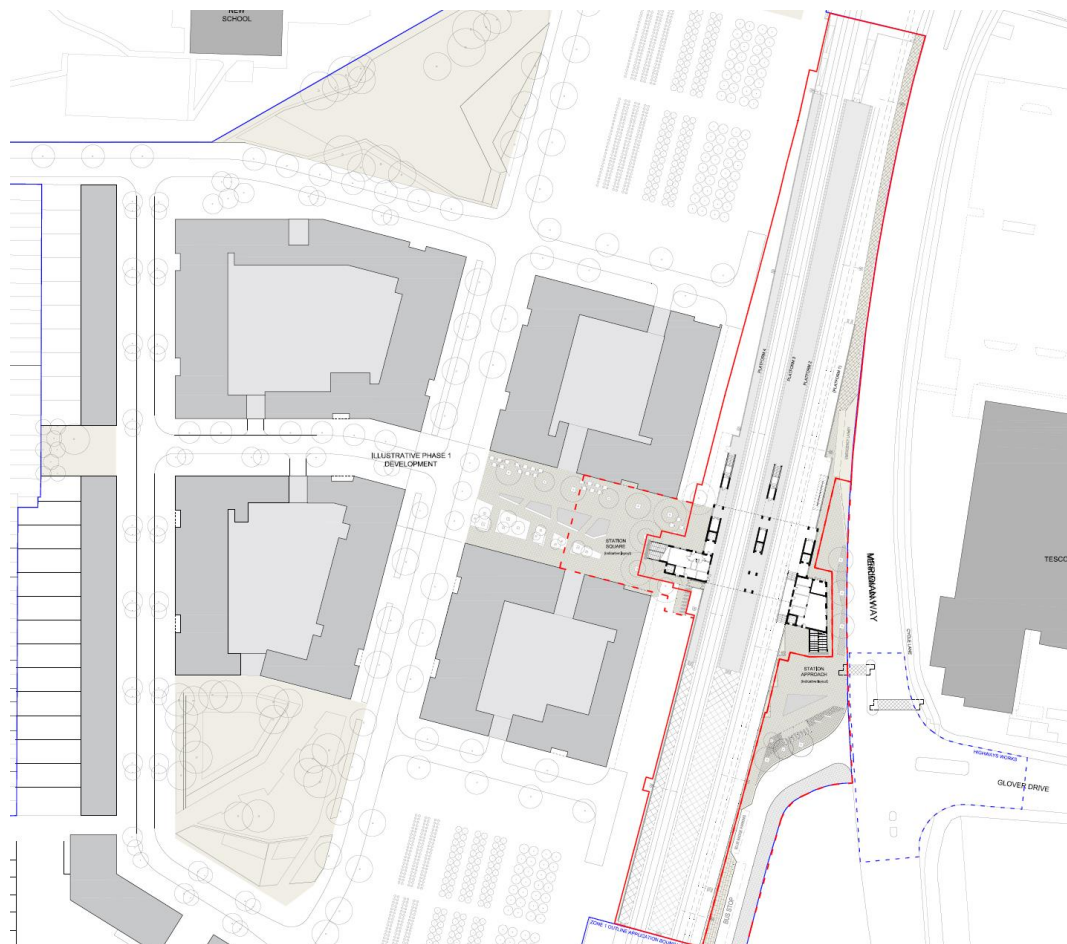


Figure 2 - Station Building Site in Context of Phase 1 Buildings

## **2. Proposal**

- 2.1 This application seeks reserved matters approval (RMA) for the detailed layout of the station platforms, stairways and buildings as well as relevant roads and landscaping within the Station Building Site pursuant to condition 5(i) of the OPP1.
- 2.2 The application does not include detailed proposals for the remaining reserved matters required by condition 5 being (ii) Scale, (iii) Appearance and (iv) Landscaping and these will be submitted as part of a separate application for RMA expected in August 2017. These matters have been separated out from layout to allow further design development of the station building whilst not impeding the ability of Network Rail to begin piling works in accordance with the programme of line closures.
- 2.3 The proposals therefore comprise the detailed layout of the station building at ground and bridge/ deck level and the position of the stairways and lifts. It also includes the position of the station platforms.
- 2.4 As part of the station proposals three 175m long platforms are proposed, 2 central platforms and 1 on the western side of the lines. Space for an additional platform on the eastern side of the railway is also safeguarded within the proposals for future provision. All platforms would be accessed from the bridge across the railway lines where the gateline for the station would be located. The station would have accommodation at ground level on both the east and west side of the railway lines with staircases and lifts on both sides providing access to the deck level. The bridge would be a publicly accessible route across the railway lines that would be available 24 hours a day.
- 2.5 Within the eastern side of the station accommodation at ground level, ancillary station service areas are provided as well as a commercial unit that would have a frontage onto Meridian Way. On the western side, further station service areas are provided as well as station staff accommodation.
- 2.6 Whilst some public realm immediately adjoining the building will be delivered as part of the station works, the wider areas of public realm as denoted by the red dashed line on Figure 2 which includes part of the Station Square, will be delivered as part of a separate construction phase but will be available at the time of station opening in accordance with the proposed phasing details provided pursuant to condition 4 of the OPP (see application reference 17/02954/CND).

### *Environmental Impact Assessment Regulations (2017)*

- 2.7 The OPP1, being EIA development was accompanied by an Environmental Statement. This subsequent application for RMA, being in general accordance with the parameters of the OPP1, is not considered to result in any additional or further significant environmental impacts that have not already been identified through the previously submitted environmental information in relation to the site. As such no further environmental information is required to be submitted with this subsequent application and the conclusions of the ES remain valid.

### 3. Relevant Planning History

#### Planning Decisions on the application site

#### 3.1 **16/01197/RE3 – Phase 1 Outline Site**

Development of Phase 1 Meridian Water comprising up to 725 residential units, new station building, platforms and associated interchange and drop-off facilities including a pedestrian link across the railway, a maximum of 950sqm retail (A1/A2/A3), floorspace, a maximum of 600sqm of community (D1) floorspace, a maximum 750sqm of leisure (D2) floorspace, associated site infrastructure works including ground and remediation works, roads, cycle-ways and footpaths, utility works above and below ground, surface water drainage works, energy centre and associated plant, public open space and childrens play areas and various temporary meantime uses without structures (landscaping and open space).

OUTLINE APPLICATION – ACCESS ONLY. EIA DEVELOPMENT.

**GRANTED – 10/07/2017**

#### 3.2 **15/04050/RE4 – Teardrop site, Meridian Way**

Remediation of contaminated soils and shallow groundwater and removal of buried structures.

**GRANTED – 26/11/2015**

#### 3.3 **15/04173/RE4 - Willoughby Lane Gas Works, Willoughby Lane**

Remediation of contaminated soils and shallow groundwater and removal of buried structures.

**GRANTED – 26/11/2015**

#### Other relevant projects

##### *West Anglia Main Line*

- 3.4 Following a number of studies which informed the OAPF and the CLAAP, a capacity issue was identified with the railway line from Angel Road to Tottenham Hale. Funding has been allocated for rail upgrades to provide extra services from Angel Road to Northumberland Park stations to Stratford via Tottenham Hale. A new third track between Stratford and Angel Road has also been agreed. Works are currently ongoing and are due for completion in 2018.

##### *North London Heat and Power Project NLHPP*

- 3.5 A Development Consent Order has now been granted for the NLHPP. The DCO covers the construction, operation and maintenance of an Energy Recovery Facility at the Edmonton EcoPark (north east of the Phase 1 Site) and will power the Lee Valley Heat Network.

#### Application Background

- 3.6 As detailed at paragraph 3.1 above, outline planning permission for the station site was granted in July 2017. The OPP secured the parameters within which the station building would come forward as well as setting out design standards in the approved Design Code (MW04 – January 2017).
- 3.7 Pre-application discussions on the detailed design of the station building that would form the reserved matters proposals began in April 2017 with the LPA. An independent panel review of the initial proposals was also held in May with the Design Council. Following these initial meetings, it was apparent that there was significant further design development required on the appearance and detailed design of the station building which could potentially jeopardise Network Rail's programme of construction works which is reliant on scheduled line closures of the West Anglia Main Line. In order to minimise disruption on the lines with further closures and a potentially delayed station opening date, it was agreed with officers that the submission of Reserved Matters in relation to the Station Building Site could be separated so design development of the superstructure could continue without delaying a start on site.
- 3.8 Therefore this application seeks reserved matters approval for the layout of the station site pursuant to condition 5(i) of the OPP. Consideration of this matter ahead of the submission of reserved matters relating to scale, appearance and landscaping required under condition 5 parts (ii)-(iv) will allow further development of these matters with officers and will ensure piling is able to commence at the end of September in accordance with Network Rail's programme thereby minimising disruption to the line.
- 3.9 The proposals therefore include the layout of the station itself including the position of the platforms, station building, station stairs and associated bridge as well as station accommodation including a retail unit and staff accommodation. This application therefore seeks to fix these aspects of the proposed design with the detailed design of the building due to be submitted under a separate application expected to be submitted by the end of August.
- 3.10 There are several pre-commencement conditions on the OPP that are required to be discharged prior to works starting on the station building site. These include details in relation to contamination and remediation of the site, construction management, ecology and phasing of works across the outline site. Details of relevant submitted applications for conditions on the OPP are listed below.

#### Relevant Applications for Approval of Details

- 3.11 **17/02954/CND – Under Consideration**  
Details required by conditions 1 (Definition of Site Extent), 4 (Phasing Details), 95 (Plan of Extents – Station Building and Public Realm), 107 (Green Procurement Plan) and 121 (Operational Waste Management) pursuant to outline planning permission 16/01197/RE3 in respect of the comprehensive redevelopment of the Phase 1 Meridian Water Outline Site dated 10/07/2017 for a mixed use



development comprising up to 725 residential units, new station building and A1-A3, D1 and D2 uses and associated infrastructure.

**3.12 17/03148/CND – Under Consideration**

Details required by conditions 9 (Ground Water Monitoring Plan – Site Wide), 10 (Remediation - Verification and Shallow Groundwater Monitoring Plan), 36 (Underlap Areas Extent), 89 (Remediation – Station Site), 90 (Remediation Strategy) and 91 (Piling Risk Assessment) pursuant to outline planning permission 16/01197/RE3 in respect of the comprehensive redevelopment of the Phase 1 Meridian Water Outline Site dated 10/07/2017 for a mixed-use development comprising up to 725 residential units, new station building and A1-A3, D1 and D2 uses and associated infrastructure.

**3.13 17/03150/CND – Under Consideration**

Details required by conditions 96 (Bat Survey – Station Building Site), 97 (Construction Environmental Management Plan – Station Building Site), 112 (Archaeology – Station Building Site), 114 (Strategy for clearance of slow worms – Station Building Site), 115 (Bat and Badger Checks – Station Building Site), 117 (Eradication strategy for invasive species – Station Building Site), 120 (Construction Waste Management Plan – Station Building Site) and 122 (Noise report from plant – Station Building Site) pursuant to outline planning permission 16/01197/RE3 in respect of the comprehensive redevelopment of the Phase 1 Meridian Water Outline Site dated 10/07/2017 for a mixed-use development comprising up to 725 residential units, new station building and A1-A3, D1 and D2 uses and associated infrastructure.

**3.14 17/03299/CND – Under Consideration**

Details required by conditions 98 (Construction Logistics Plan – Station Building Site) and 106 (telecommunications strategy – station building site) pursuant to outline planning permission 16/01197/RE3 in respect of the comprehensive redevelopment of the Phase 1 Meridian Water Outline Site dated 10/07/2017 for a mixed-use development comprising up to 725 residential units, new station building and A1-A3, D1 and D2 uses and associated infrastructure.

**4. Consultations**

**4.1 Pre-Application**

4.1.1 Prior to the submission of the application the applicant sought pre-application advice from the LPA. A series of meetings was held with officers to review and develop the proposals. As part of the pre-application process the proposals were also presented to an independent design review panel facilitated by the Design Council on 4<sup>th</sup> May 2017.

4.1.2 The comments of the panel are summarised below:

- The location of the station and the principle of a dual purpose bridge providing public access over the railway line and passenger access to the station is an effective approach to connectivity and access within the masterplan.



- Concern that tight timescales are driving the delivery of an under-ambitious design.
- Encourage the project team to identify key technical and engineering aspects of the design that need to be agreed/ approved and implemented at an earlier stage in order to allow for the design of the station building superstructure to be developed and relate better to the future development of the area and help shape the public realm around it.
- The station requires greater architectural presence reflective of its status as an important first step in the regeneration scheme.
- It should be redesigned to have prominence and a civic character despite having a smaller massing than its surroundings.
- Encourage focus on artistic expression in the architecture and in integral details such as lighting, street furniture stairs and use of materials.
- The principle of a dual function bridge providing passenger access to the station and public access over the train line is the right approach in our view.
- The width of the bridge seems appropriate however there is concern that the height of the wall to the southern side of the bridge will create an overly enclosed 'tunnel-like' effect, lacking outward visibility or public surveillance.
- A more visually permeable material or design solution should be adopted to open this space up. Stairs down to the station platforms should also be designed to maximise visual openness.
- Question whether the stairs on the east are landing in the right place and whether there is an opportunity to straddle Meridian Way and thereby overcome the physical barrier of this road to east west permeability.
- Question whether the placement of the lifts behind the stairs create an unwelcoming and awkward route.
- Back-up options should be explored for when lifts are broken.
- Station bridge does not appear to sufficiently promote the route to the east through future phases of the masterplan along Glover Drive.
- The proposals anticipate a significant change to the road environment along Meridian Way and the scheme should demonstrate how it will transform the existing road environment.
- More thought could be given to the movement of cyclists across the railway and whether a ramp or tunnel could facilitate this.
- The scheme should do more to activate the adjoining public spaces.
- Further consideration should be given to how the station will be integrated with other modes of transport including buses, taxis and cycles.
- An increased number of cycle spaces should be provided in the public realm.

#### *Officer Response*

*As a result of the panel review and officer pre-application advice, the design of the station building was revisited with several key elements undergoing significant changes from the scheme that was presented to the panel and early meetings to the LPA. The siting and orientation of the station building remains largely unchanged given it is responding to specific site constraints such as the position of the existing railway lines, availability of land for platforms and the principles and parameters set out in the OPP. However, it was acknowledged*

*that further design development was required on the superstructure and detailed design of the building. As such, this has continued to be developed by the project architects and has been subject to further pre-application discussions with officers and is also scheduled for a further review by the Design Council's panel on 5<sup>th</sup> September. The detailed design will form the basis of a future reserved matters application expected to be submitted in September and a full assessment of the design and how it responds to earlier criticisms will be undertaken by officers at that time. Consideration of the panel's comments insofar as they relate to layout, the matter sought for approval in this application, is provided at sections 6.2-6.4 of this report. Some illustrative designs of the building are included at section 6.3 of this report to provide members with an indication of how the design has been developed and what the layout is ultimately proposed to support.*

## **4.2 Statutory and non-statutory consultees**

### Environment Agency

- 4.2.1 No objections to the application subject to the principles and requirements set out in the conditions on the outline application being adhered to.

### London Borough of Haringey

- 4.2.2 No objections raised.

### Metropolitan Police (Secure By Design)

- 4.2.3 No objections to the proposed layout. Request that proposed commercial outlets comply with the Secured by Design Commercial Development Guide 2015.

### Transport for London – see applicant's response for additional details

- 4.2.4 TfL have made the following comments in response to the consultation:

1. TfL welcomes the proposed 2045 Full Masterplan bus proposal (as illustrated in the Design and Access Statement) however, from TfL's point of view this is not a fixed strategy and TfL's agreement would be required prior to implementation. TfL are working on wider bus strategies which would also need to feed into it. TfL and the developer would need to collaborate on this. TfL also requests that the route numbers are omitted from the drawings to avoid any confusion as this is still being decided and subject to change.

### *Officer Response*

*The bus connections shown in the DAS submitted with the application relate to the wider Meridian Water Masterplan area as designated within the ELAAP and are beyond the scope of the RMA in relation to layout of the station. The proposals are included to provide contextual information regarding future connections to the station site and the phase 1 site. The route network shown is*

*noted as conceptual and subject to agreement. Therefore, it is not considered necessary to amend the DAS in relation to the current application given the status of this information as illustrative and as a supporting document. Any route changes would be subject to discussion and agreement with TfL however these fall outside of the scope of this current RMA.*

2. The Design and Access Statement proposes a new bus stop to the south of the station as part of the Phase 1 development yet it has not been agreed with TfL. It also appears as though the new bus stopping facility acts as a taxi rank, and presumably also staff facilities. TfL would be expected to be engaged on concept and details of TfL's services and regulatory functions.

*Officer Response*

*The bus stop and taxi rank are shown indicatively outside of the red line of the Station Building Site as context for access to the station. Detailed proposals for the wider outline site will be brought forward separately by the Master Developer and these will be subject to consultation and agreement with TfL. Due to the modelled impacts on highway network performance as a result of an additional arm on the Meridian Way/ Glover Drive junction detailed in the original Transport Assessment, it was agreed with TfL that a bus stop was unnecessary and would not be provided as part of the Phase 1 development.*

3. TfL welcomes the proposal to incorporate the Legible London wayfinding programme to the site and within the station.

4. Further information is required as to how staff car parking spaces will be monitored e.g. only for staff.

*Officer Response*

*This information is required to be provided by condition 142 (Station Access Road Management Plan) on the outline planning permission.*

5. The application suggests that a taxi drop off and pick up area will be provided outside of the station. Will there be a taxi rank here and will this area include any taxi electric charging facilities? As outlined in the draft Mayors Transport Strategy, in order to succeed in making the transition to zero emission, the charging infrastructure across London will need to change. This includes meeting the need for rapid charging to support Zero Emission Capable taxis.

*Officer Response*

*This detail is outside of the scope of this RMA but the comments are noted for future details and RMA's that will provide this information.*

6. The provision of 32 covered and secure public spaces is noted. TfL would expect that cycle parking would be available and accessible on opening of the station. Details of the type of cycle storage is omitted from the plans. TfL's guidance on cycle parking provision, London Cycling Design Standards, should

be considered. It is also queried how the applicant would promote cycling to and from the station e.g. a cycle hire scheme?

#### *Officer Response*

*Condition 140 (Cycle parking details – Station Public Realm Site) requires full details of cycle parking for the station to be provided including type of rack and spacing. The condition requires the cycle parking to be provided prior to operation of the station to ensure they will be available and accessible from station opening. As some cycle parking will be provided within the Main Site area, an additional condition has been imposed to ensure cycle parking facilities are provided prior to operation of the station on the western side of the station within the public realm. In addition, details of cycle gutters to be provided on both sets of the station stairs are required to be submitted in accordance with the recommended conditions in section 8 of this report.*

#### *Applicant Response*

*Network Rail have limited scope to actively promote cycling given the limited public realm within their control. However, lifts to and from the station bridge deck, and to platforms are designed to accommodate bicycles. A wide access ticket gate will facilitate cycle access into the ticketed part of the station.*

7. Further information is required as to the planned closure of the station at Angel Road and when this will be advertised to the public.

#### *Applicant Response*

*Angel Road Station will be closed in line with the opening of Meridian Water Station in 2019 to provide continuity of service to the area. Timelines for informing the public are not yet confirmed, but will be undertaken in line with the Railways Act 2005.*

8. The application states that the proposal to install two lifts on either side of the bridge was rejected on financial grounds. TfL wishes to see the Cost Benefit Analysis and how the station design takes account of the 2045 Masterplan in making this decision.

#### *Applicant Response*

*The station (including the number of public lifts) is designed to meet predicted pedestrian flow and passenger numbers out to 2075, including the impact of Crossrail 2. The additional design and build costs of additional lifts are not allowed for in the funding agreements for the station and the operational costs and the maintenance burden are significantly over and above that allowed for in the standard station lease arrangements with the train operating company.*

#### *Officer Response*

*Officers have sought to explore with the applicant what provisions could be put in place should there be a lift failure on either or both sides of the bridge to ensure continued public access is possible across the railway lines. Further assessment of this is provided in section 6.3 below.*

9. Prior to construction of the station a Construction Logistics Plan would need to be produced. It is important that disruption to the transport network is kept to a minimum.

*Officer Response*

*The submission of a CLP is already a requirement of condition 98 of the OPP and has been submitted to the LPA for consideration as detailed at paragraph 3.14 above.*

10. The Mayor has proposed in his draft Transport Strategy that Crossrail 2 will be open by 2033. Therefore, the application needs to confirm that the station design now proposed is in line with this aspiration.

*Applicant Response*

*The Crossrail 2 team have been consulted by the Network Rail project team at various stages of the design development process. The station design explicitly provides space for a fourth platform including stairs and lift, and additional entrances/ exits that can be opened up to accommodate increased passenger numbers anticipated in line with Crossrail 2. Pedestrian flow modelling used in the station design takes account of anticipated Crossrail 2 traffic out to 2075. A memorandum of understanding has been signed between the Lee Valley Rail Programme (including Meridian Water Station) and Crossrail 2 which governs the extent to which the Lee Valley Rail Programme will accommodate Crossrail 2 in its design.*

11. The Design and Access Statement states “Responses to LPA feedback on the station design flowing from the Design Council CABE review on 4<sup>th</sup> May 2017”. How does the design respond to this review? TfL’s interest is strategic in making sure the design responds to the wider regeneration.

*Officer Response*

*This is addressed in section 6.3 below.*

Traffic and Transport

4.2.5 *Pedestrian and cyclist access*

The proposed access arrangements are acceptable for the purposes of this application given further details of the operation, design and materials will be provided in the discharge of conditions 102 (Public Realm Strategy – Station Building Site), 124 (Public Access – Station Building Site) and 141 (Highway and Footway Details – Station Public Realm Site). There is a suggested

improvement to the design which would leave at least 3.8m for passive provision for a potential bridge/ ramp across Meridian Way; this is strongly supported as it will allow future flexibility when looking to provide direct and safe access to the Causeway and wider Meridian Water development.

#### *Vehicular access*

In line with the outline consent, the vehicular access via the station access road will be limited to certain users (station staff, disabled and deliveries) with the management arrangements to be set out when discharging condition 142 (Station Access Road Management Plan). Any vehicular access required from the west of the station site will be dealt with via the discharge of conditions relating to the remaining phase 1 site.

#### *Cycle parking*

Whilst the level of cycle parking appears low it is noted that this will be addressed by discharge of condition 140 (Cycle parking details – Station Public Realm Site). There is no dedicated onsite parking for staff. The applicant has stated that they do not require dedicated cycle parking spaces for their staff given low staff numbers and secure parking that will be available in the public realm. This is accepted.

#### Environmental Health

- 4.2.6 No objections.

#### Natural England

- 4.2.7 No comments.

### **4.3 Public Consultation**

- 4.3.1 The application was advertised in the local press on 19/07/2017.
- 4.3.2 The application was advertised on land adjoining the site from 11/08/2017-01/09/2017.
- 4.3.3 No responses have been received to date.

### **5. Relevant Policy**

- 5.1 In determining the application the Council has the following main statutory duties to perform:
- To have regard to the provisions of the development plan, so far as material to the application and to any other material considerations (Section 70 (2) Town and Country Planning Act 1990); and

- To determine the application in accordance with the development plan unless other material considerations indicate otherwise (Section 38(6) of the Planning and Compulsory Purchase Act (2004)).

### The Development Plan

- 5.2 The development plan for the area comprises the London Plan 2016 and the London Borough of Enfield's Core Strategy (2010) and Development Management Document (2014).
- 5.3 The following principle national, regional and local development plan policies are relevant to the application:

National Planning Policy Framework (NPPF) (2012)

National Planning Policy Guidance (NPPG)

### The London Plan 2016

Policy 2.13 – Opportunity areas and intensification areas  
 Policy 2.14 – Areas for regeneration  
 Policy 3.1 – Ensuring equal life chances for all  
 Policy 3.2 – Improving health and addressing health inequalities  
 Policy 3.3 – Increasing housing supply  
 Policy 3.4 – Optimising housing potential  
 Policy 3.5 – Quality and design of housing developments  
 Policy 3.6 – Children and young people's play and informal recreation facilities  
 Policy 3.7 – Large residential developments  
 Policy 3.8 – Housing choice  
 Policy 3.9 – Mixed and balanced communities  
 Policy 3.12 – Negotiating affordable housing  
 Policy 3.13 – Affordable housing thresholds  
 Policy 3.15 – Coordination of housing development and investment  
 Policy 3.16 – Protection and enhancement of social infrastructure  
 Policy 4.8 – Supporting a successful and diverse retail sector  
 Policy 4.12 – Improving opportunities for all  
 Policy 5.1 – Climate change mitigation  
 Policy 5.2 – Minimising carbon dioxide emissions  
 Policy 5.3 – Sustainable design and construction  
 Policy 5.4A - Electricity and gas supply  
 Policy 5.5 – Decentralised energy networks  
 Policy 5.6 – Decentralised energy in development proposals  
 Policy 5.7 – Renewable energy  
 Policy 5.9 – Overheating and cooling  
 Policy 5.10 – Urban greening  
 Policy 5.11 – Green roofs and development site environs  
 Policy 5.12 – Flood risk management  
 Policy 5.13 – Sustainable drainage  
 Policy 5.14 – Water quality and waste water infrastructure  
 Policy 5.15 – Water use and supplies



Policy 5.21 – Contaminated land  
Policy 6.2 – Providing public transport capacity and safeguarding land for transport  
Policy 6.3 – Transport capacity  
Policy 6.9 – Cycling  
Policy 6.10 – Walking  
Policy 6.12 – Road network capacity  
Policy 6.13 – Parking  
Policy 7.1 – Lifetime neighbourhoods  
Policy 7.2 – An inclusive environment  
Policy 7.3 – Designing out crime  
Policy 7.4 – Local character  
Policy 7.5 – Public realm  
Policy 7.6 – Architecture  
Policy 7.7 – Location and design of tall and large buildings  
Policy 7.8 – Heritage Assets and archaeology  
Policy 7.14 – Improving air quality  
Policy 7.15 – Reducing noise and enhancing soundscapes  
Policy 7.18 – Protecting local open space and addressing local deficiency  
Policy 7.19 – Biodiversity and access to nature  
Policy 7.21 – Trees and woodlands  
Policy 7.28 – Restoration of the blue ribbon network  
Policy 8.2 – Planning obligations  
Policy 8.3 – Community infrastructure levy

#### Enfield Core Strategy (2010)

Core Policy 1: Strategic growth areas  
Core policy 2: Housing supply and locations for new homes  
Core policy 3: Affordable housing  
Core Policy 4: Housing quality  
Core Policy 5: Housing types  
Core Policy 6: Housing need  
Core Policy 17: Town Centres  
Core Policy 20: Sustainable Energy use and energy infrastructure  
Core Policy 21: Delivering sustainable water supply, drainage and sewerage infrastructure  
Core Policy 24: The road network  
Core Policy 25: Pedestrians and cyclists  
Core Policy 26: Public transport  
Core Policy 28: Managing flood risk through development  
Core Policy 29: Flood management infrastructure  
Core Policy 30: Maintaining and improving the quality of the built and open environment  
Core Policy 32: Pollution  
Core Policy 34: Parks, playing fields and other open spaces  
Core Policy 36: Biodiversity  
Core Policy 37: Central Leaside  
Core Policy 38: Meridian Water  
Core Policy 46: Infrastructure Contributions

#### Enfield Development Management Document (2014)

DMD1: Affordable Housing on Sites Capable of Providing 10 units or more  
DMD3: Providing a Mix of Different Sized Homes  
DMD6: Residential Character  
DMD8: General Standards for New Residential Development  
DMD9: Amenity Space  
DMD10: Distancing  
DMD25: Locations for new retail, leisure and office development  
DMD28: Large local centres, small local centres and local parades  
DMD37: Achieving High Quality and Design-Led Development  
DMD38: Design Process  
DMD39: Design of Business Premises  
DMD42: Design of Civic/ Public Buildings and Institutions  
DMD43: Tall Buildings  
DMD45: Parking Standards and Layout  
DMD46: Vehicle Crossovers and Dropped Kerbs  
DMD47: New Road, Access and Servicing  
DMD48: Transport Assessments  
DMD49: Sustainable Design and Construction Statements  
DMD50: Environmental Assessments Method  
DMD51: Energy Efficiency Standards  
DMD52: Decentralised Energy Networks  
DMD53: Low and Zero Carbon Technology  
DMD55: Use of Roofspace/ Vertical Surfaces  
DMD56: Heating and Cooling  
DMD57: Responsible Sourcing of Materials, Waste Minimisation and Green Procurement  
DMD58: Water Efficiency  
DMD59: Avoiding and Reducing Flood Risk  
DMD61: Managing Surface Water  
DMD64: Pollution Control and Assessment  
DMD65: Air Quality  
DMD68: Noise  
DMD69: Light Pollution  
DMD72: Open Space Provision  
DMD79: Ecological Enhancements  
DMD80: Trees on development sites  
DMD81: Landscaping

#### 5.4 Other Relevant Policy

Upper Lee Valley Opportunity Area Planning Framework (OAPF)

Proposed Submission Edmonton Leaside Area Action Plan 2017 (ELAAP)

Meridian Water Masterplan (MWM)

## **6. Analysis**

### **6.1 Principle of Development**

- 6.1.1 The principle of a new station at Meridian Water to replace the existing station at Angel Road on the West Anglia Main Line was established through the OPP for Phase 1 Meridian Water, granted in July 2017. This established the location and general footprint and scale parameters for a new station building on the western side of Meridian Way as part of the residential led mixed use redevelopment of the wider Phase 1 site.
- 6.1.2 The proposed station will greatly improve transport connections in the area for future occupiers and existing residents. In addition, it will provide a new public east-west link across the railway lines connecting existing residential communities in the west to the future planned development on land to the east as part of the aspirations of the MWM.
- 6.1.3 The application seeks RMA for the layout of the station building and its platforms on what is defined in the OPP for Phase 1 as the 'Station Building Site' (see application ref: 17/02954/CND). A further RMA will be submitted for the detailed design of the station building to complete the matters reserved for approval on the Station Building Site which will form the first phase of development on the Phase 1 site.
- 6.1.4 Adjoining areas of public realm to the east of the station building will come forward as a separate phase of development by the Master Developer for the Phase 1 site. This is defined as the Station Public Realm Site. The phasing plan submitted to discharge planning condition 4 of the OPP demonstrates that access to the station and the adjoining areas of public realm will be constructed and available at the time of station opening to ensure safe and convenient access to the station.
- 6.1.5 The area to the west of the site which largely comprises residential development and a public square is defined as the Main Site through conditions attached to the OPP. This would form a distinct phase of development to be delivered by the Master Developer. However, similar to the Station Public Realm, part of the square and relevant pedestrian access from streets to the west will be constructed and made available at the time of station opening as detailed on the submitted phasing plan (as provided in relation to application ref: 17/02954/CND).
- 6.1.6 The principle of development is therefore established and accepted and the information submitted to the LPA to discharge various conditions of the OPP ensure that surrounding areas of public realm will be delivered concurrently ensuring safe and convenient access.

### **6.2 Compliance with Outline Planning Permission**

- 6.2.1 Outline consent was granted for the station subject to a set of parameters as well as a design code. RMA's are required to be in accordance with these controls to

ensure that the development does not go beyond the scope of what was assessed at the outline stage.

- 6.2.2 The application is accompanied by an assessment of how the proposals for the layout of the station comply with the outline parameters. These largely demonstrate that the proposals accord with relevant parameters as detailed below.

#### *Development Zones and Building Dimensions*

- 6.2.3 This plan sets out the specified zones for development with allowable limits of deviation. It defines maximum building heights for each zone as well as no build zones. It also sets out the areas required to accommodate steps, ramps and platforms to provide access to the station.
- 6.2.4 The layout drawings submitted demonstrate that the proposed footprint of the station building would be within the specified development zone for the station (Zone F). As the application relates to layout only, the height of the building is not specified within the submission and this will be assessed when the detailed designs are submitted for approval.

#### *Public Realm and Land Use*

- 6.2.5 This plan sets out the main areas of access and circulation and public realm around areas of built development including pedestrian only spaces and areas to access to station. It includes a zone for an ecological corridor that runs alongside the west side of the railway lines save for a break around the station stairs.
- 6.2.6 The proposed layout plans are compliant with these parameters showing that the building would sit within the designated build zone respecting the areas required for public realm and circulation.

#### *Privacy Buffers and Distancing*

- 6.2.7 This plan sets out the key distances required between zones as well as setting out the locations and distances across residential courtyards as well as defensible space around buildings.
- 6.2.8 This plan requires that the station stairs on the western side would not be within 5m of Block B to the north, or 6m to Block D to the south. The proposed plans show that the station stairs would be approximately 12m from Block B's building zone, and 6m from Block D's building zone therefore in accordance with the parameters.

#### *Vehicular Access and Route Network*

- 6.2.9 This plan identifies the details associated with vehicular accessibility to and within the development in terms of the positioning of access and circulation routes and how these fit into the surrounding network.

These areas largely fall outside of the red line of the RM for the Station Building Site. However, the plans submitted which include the surrounding context show that the proposals would not conflict with these parameters.

#### *Pedestrian Access and Route Network*

- 6.2.10 This parameter plan sets out pedestrian and cycle routes and their relationship to the accesses into and out of the site.
- 6.2.11 This plan seeks to ensure pedestrian route is provided across the railway lines through the station bridge. The proposed layout for the station includes this provision.

#### *Development Use at Ground Floor Level*

- 6.2.12 This parameter plan sets out the location and use of ground floor frontages. It seeks to secure “predominantly retail (A1-A3)” frontages facing onto the square to the west of the station building to activate this public space as shown on Figure 3 below.

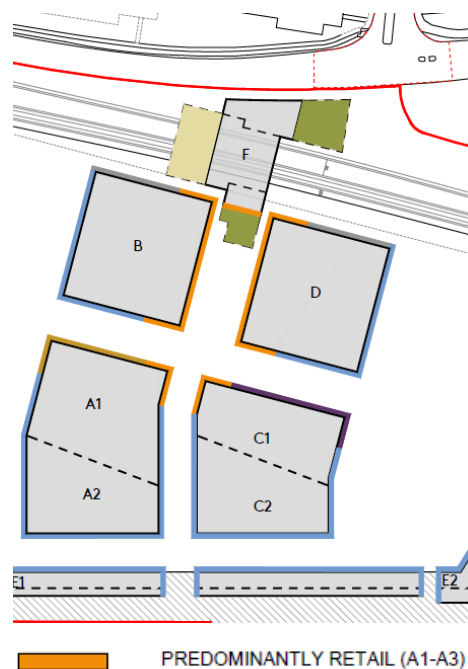


Figure 3 - Development Use at Ground Floor Level Parameter (Extract)

- 6.2.13 The proposal for the layout of the station does not include any retail frontage on the western frontage of the station and therefore represents a deviation from the OPP in this respect. The proposed layout for the western side of the station building is shown in Figure 4 below. This shows the staircase would directly front the square with the remaining façade forming ancillary station accommodation alongside the platform. Staff accommodation is proposed below the station stair.

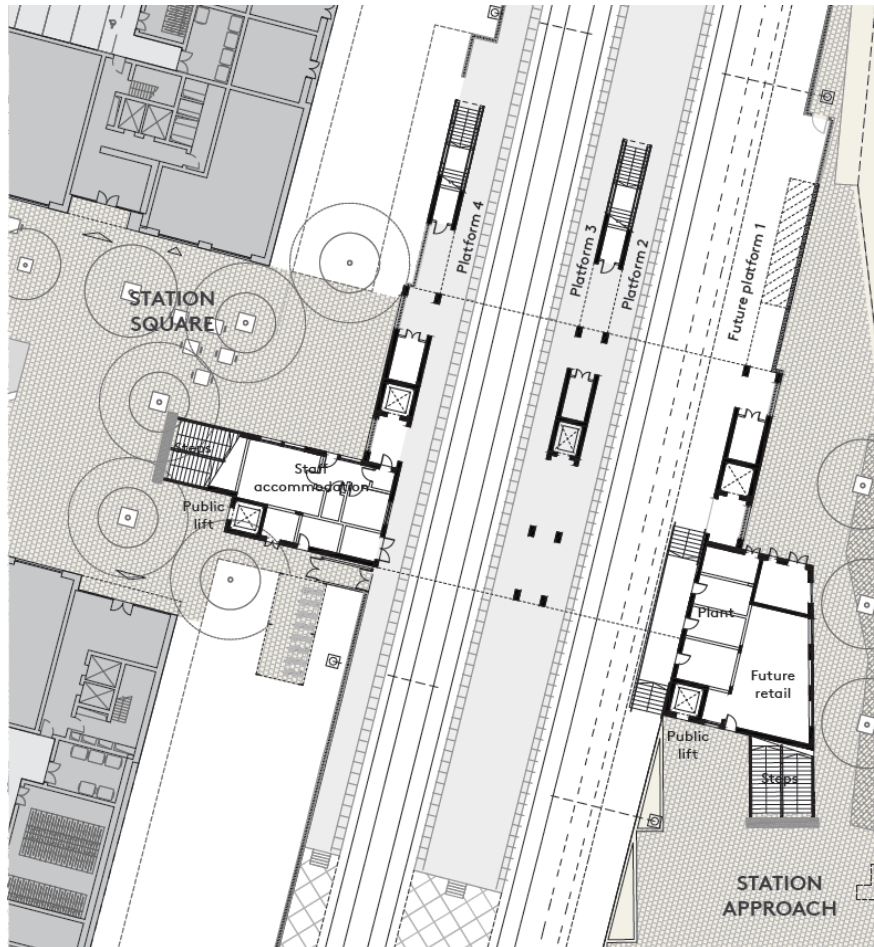


Figure 4 – Station Layout (Ground/ Platform Level)

- 6.2.14 The loss of the retail frontage from the western elevation has resulted from a change in the originally envisaged stair position and the operational requirements to provide staff accommodation on the western side, rather than the eastern side. In addition, passive provision for future direct access to the western platform (platform 4) from the square would compromise the opportunity for this to happen in the future.
- 6.2.15 To reduce any impacts of this loss on the level of activity and surveillance of the square an improved public realm strategy for this end of the square has been included within the submission in the Design and Access Statement Addendum (280-A-REP-AD01). This includes the design of the western station frontage which has been developed to provide a series of future openings at ground floor level to improve the relationship with the square and provide an attractive, overlooked and active space in this part of the public realm. Whilst the detail of the western frontage will come forward with the 2<sup>nd</sup> RMA for the design of the station, an illustrative sketch is provided at Figure 5. This shows the proposed openings would be infilled with railings and screens until such time that direct access to the platform is required. Notwithstanding this arrangement, the railings on the west elevation together with the windows serving the staff accommodation beneath the staircase on the north elevation would provide some transparency



and interaction with the adjoining public realm and provide some overlooking of the space.



Figure 5 - Sketch view of illustrative proposals for western elevation and station square



Figure 6 - Potential direct access to platform 4 from Station Square



- 6.2.16 The detailed design of the public realm in the station square will be brought forward by the Master Developer. However, the figures above provide an indication of how the space could be treated to ensure an active and attractive space. The Design Code accompanying the OPP contains a section on the public realm to ensure a high-quality space is provided in keeping with the above images.
- 6.2.17 Given the future relationship with the square as illustrated above together with the requirement for predominantly retail frontages to be provided on the ground floor of the adjoining Development Zones B and D, the proposed deviation from the approved parameter is not considered to compromise the levels of activity in the square or the quality of this area of the public realm. The proposed deviation is therefore acceptable.
- 6.2.18 Further parameter plans relating to Development Uses at First and Upper floor levels are not relevant to the consideration of this application or proposals for the station buildings. Similarly, parameters in relation to public space provision and protected frontages relate to land outside of the station building site and are not relevant to the consideration of the layout of the station. Although it is noted that the proposals subject of this application would not compromise the delivery of any of these matters.

### 6.3 Design

#### *Layout*

- 6.3.1 The station is located approximately 250m south of the existing Angel Road station with the new platforms proposed to be built alongside the existing tracks. This location was established during consideration of the OPP being the optimum position having regard to land availability, track and platform alignment, position of overhead line equipment and accessibility for passengers. Notably, a clear 6m distance is required between the overhead line equipment supports and the station stairs that land on the platforms. As such, the public footbridge and public stairs that land on the east and west of the bridge are dictated by this constraint along with the land required for the platforms.
- 6.3.2 It is acknowledged that the eastern stair does not directly correspond to the existing alignment of Glover Drive or the pedestrian crossing across Meridian Way as commented on by CABA at the design review. However, the constraints discussed at 6.3.1 do not allow the station to be positioned any further south as the available land tapers reducing the ability to provide the necessary platform space. Notwithstanding this, the stairs are considered to sufficiently promote the route to the east as well as relating well to the public realm on this side of the station. The stairs as they land have a southerly orientation opening directly onto the public realm of Station Approach as well as being in close proximity to the crossing point on Meridian Way (Figure 7).

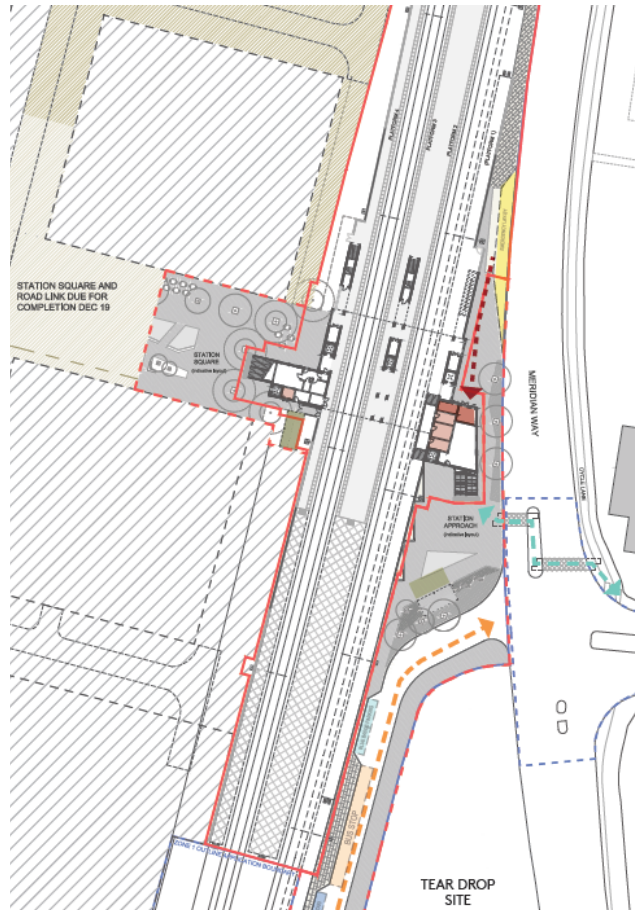


Figure 7 - Relationship of eastern stair with Meridian Way Crossing

- 6.3.3 The panel also commented on the relationship of the route with the busy traffic environment of Meridian Way and suggested that the layout could be improved if the stairs straddled Meridian Way landing on its eastern side. This is not however a requirement of the OPP and is not included within the provisions of the consent. Officers recognise that Meridian Way is not a calm pedestrian environment at present being a significant arterial route through the industrial areas of Enfield. However, the change in the nature of the road is something that is recognised as a wider strategic issue that will require a number of area-wide measures throughout the various phases of the Meridian Water development. These include proposed changes to the balance of land uses in the Meridian Water area, encouraging modal shift, using waterways as transportation and balanced parking provision as well as changes to movement patterns and desire lines (see chapter 8, ELAAP).
- 6.3.4 The environment of Meridian Way is therefore a wider matter that falls outside of the scope of the current RMA for the layout of the station. However, the layout does help promote the aims of the draft ELAAP and its overall vision by supporting improved access to the station (pedestrian and cycle) and promoting east-west movement. The current vision for the Causeway connection from the phase 1 site does not include a bridge link across Meridian Way to Glover Drive.

That being said, if on further development of the wider Meridian Water Masterplan the Council consider this to be an appropriate option the station has been designed so as not to compromise the ability to achieve this in the future (see Figure 8 below).

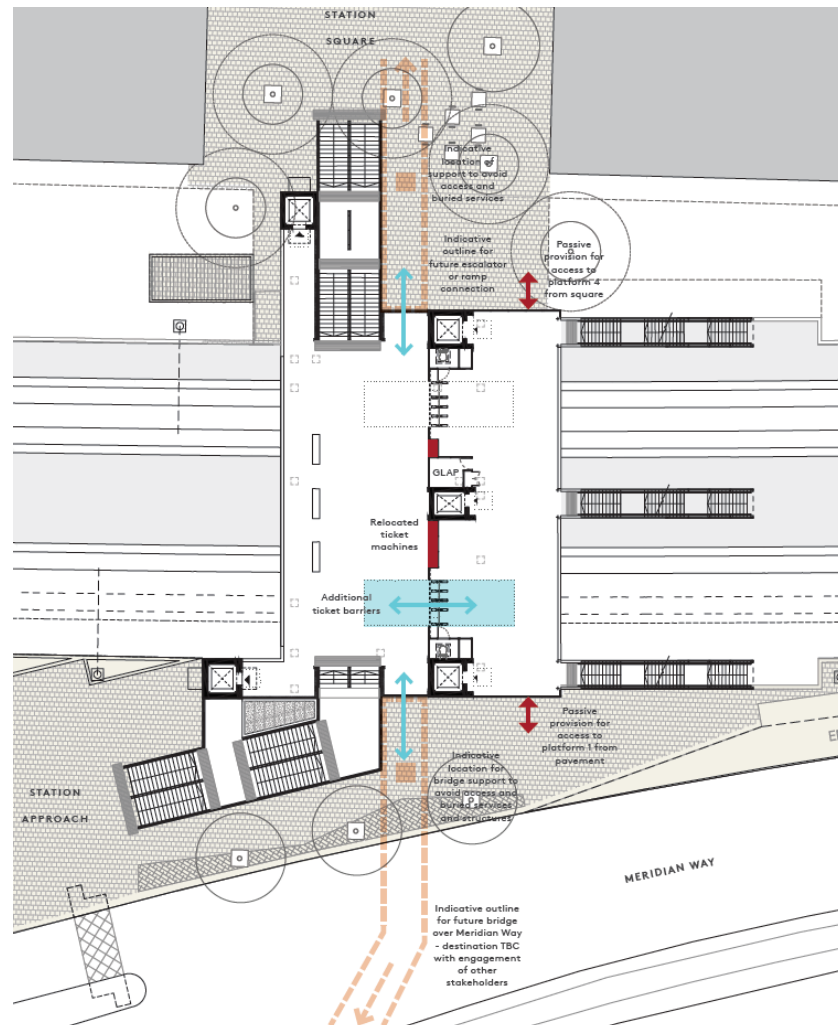


Figure 8 - Safeguarding of potential bridge across Meridian Way

- 6.3.5 There is limited accommodation at ground level within the station building itself. On the eastern side, the area beneath the public stairs accommodate a retail unit (occupier yet to be identified), a substation, bin store and plant rooms as well as a public lift. The retail unit would have its main frontage facing east towards the foot path / Station Public Realm and Meridian Way with its entrance directly from Station Approach to the south adjoining the stair case. This provides the unit with maximum visibility and is considered, subject to the detailed design of the façade to provide good levels of activity and surveillance of the adjoining areas of public realm. The lift is also in close proximity to the stairs so is in the direct line of sight of the main desire line to the station for all users.
- 6.3.6 On the western side of the station the stairs lead directly onto the square and as such there is limited scope for the provision of active frontages on this side of the

station. However, the treatment of the station façade as well as the future design of the public realm as discussed at 6.2.12-6.2.17, is considered to provide sufficient levels of activity when combined with the future commercial frontages of the adjoining blocks. Similar to the eastern side, the public lift adjoins the main stair case accessed directly from Station Square thereby providing good visibility of this facility also within the main desire line to the station bridge.

- 6.3.7 The bridge deck provides 24-hour public access across the railway as well as a secure gateline entry to the station itself. The public route would be a minimum of 8m wide in accordance with the approved OPP Design Code. Some bench seating is also proposed along the bridge to provide places to dwell and rest along the route. The bridge has been designed to minimise blind corners so that clear lines of sight are provided across the bridge in the interest of safety and security. Whilst the lifts at bridge level may appear enclosed by the balustrading, the detailed design of the balustrading will ensure maximum visibility in these areas.
- 6.3.8 The station layout has been designed to accommodate the predicted number of passengers in the future masterplan scenario of the wider project area. It has also been future proofed to ensure additional capacity can be easily built into the design to absorb service improvements, train formation improvements, increased entry/ exit numbers and improved access.
- 6.3.9 Space has been made available for the construction of a fourth platform on the eastern side of the station (denoted as Platform 1 on the plans). Although not intended to be built out at this stage, the land will be safeguarded and passive space for platform stairs and lift shaft to this platform are included in the design. This platform is to allow for enhanced capacity should Crossrail 2 require it. Whilst the future-proofing of the station is welcomed to take account of the needs of Crossrail 2 officers are required to ensure that the appearance of the station in the interim is satisfactory. As such a condition has been imposed requiring further details of the interim treatment of the platform and associated access areas.

#### *Inclusive Access*

- 6.3.10 The proposed station layout is considered to provide a legible and accessible environment for all users. The provision of public lifts ensures step free access is provided across the bridge and their position is such that they are legible and being as close to the staircase as possible, providing a direct, convenient and inclusive route from the public realm. All lifts across the station (including those to the station platforms) are 16 person through lifts and are able to accommodate bicycles and wheelchairs.
- 6.3.11 Only 1 public lift is provided on each side of the station and as such in the event of lift failure on one or both sides of the bridge, a step-free route across the railway lines would not be possible. The applicant has stated that it would not be possible to provide additional lifts at either side of the station due to cost and station design implications. In the event of lift failure, the applicant has provided an alternative access strategy across the lines that would involve using platforms

lifts and platform lengths to get across the lines. When gaining access from the west, this would involve a 50m detour and from the east a 380m detour (see Figure 10 below). This would only be possible during station opening hours as it involves accessing platforms and lifts that are within the controlled station area. Whilst not an ideal scenario, the cost and programme implications to Network Rail are acknowledged and the provisions for alternative access in the eventuality of both lifts failing are considered to adequately address concerns raised by officers and consultees. The alternative access arrangements are secured by condition.

6.3.12 Stepped access to the bridge is 5m wide and provided with a central as well as side hand rails at both 600mm and 900mm high throughout. Large landings are provided to provide moments for pause or rest. Treads would be contrasting with tactile finishes to tops and bottoms of stairs for the visually impaired. Full details of the materials are required to be provided as part of future reserved matters relating to landscaping (condition 5, part iv).

6.3.13 To facilitate bicycle access over the bridge, a cycle channel is proposed on the stairs so that bicycles can be wheeled up and down the stairs. An indicative image of how this could be designed is provided below at Figure 9. This shows how the provision of an integrated cycle gutter tilted at a 30 degree angle allows the bicycle to be tilted so as to avoid catching pedals on the lower handrail. Full details of this channel are secured by condition to ensure this is the best option and does not result in additional hazards for the mobility impaired using the stairs.

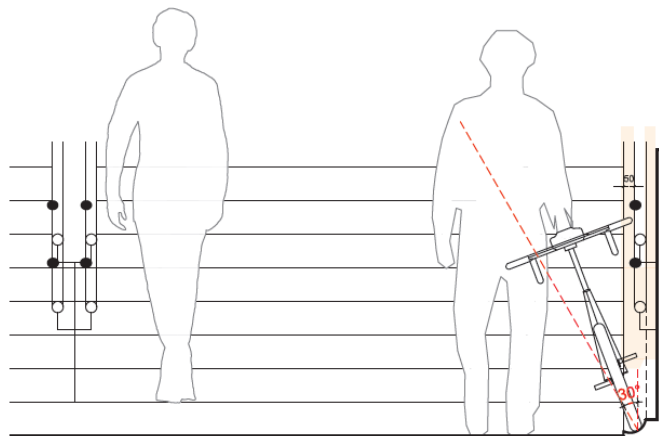


Figure 9 - Cycle Gutter Provision

6.3.14 Level access from the platforms to the trains will be provided by movable ramps operated by station staff as is the existing arrangement at many existing Network Rail stations. The provision of more permanent raised areas or humps to access the station is not possible at this time given the different types of trains/ rolling stock of varying lengths and formations that will serve the station. However, the provision of level access is not precluded and could be introduced by the train operator at this and other stations should they choose to do so in the future.

#### *Appearance*

6.3.15 As outlined earlier in this report the detailed design of the superstructure will be submitted under a separate application for Reserved Matters Approval in relation to Scale, Appearance and Landscaping (required by condition 5, parts i-iv of the OPP). Pre-application discussions have been ongoing with officers since pre-application advice was first given in March 2017 and following the comments made by the independent panel review. These have been to develop the detailed design and address the concerns raised with the design quality of the station building. Significant design development has been progressed and as such the formal application is due to be submitted to the LPA in early September 2017, shortly before this application for the station layout is due to be reported to committee.

6.3.16 Whilst some of the more detailed elements such as façade and balustrade design and materials are yet to be fully resolved, officers are satisfied that the determination of the station layout would not prejudice the ability to achieve a high-quality form and finish to the superstructure that would have the presence required of this key part of the wider Meridian Water Regeneration. As officers are seeking delegated authority to determine future reserved matters in relation to the Station Building Site some draft images of the station are provided below to illustrate how the superstructure could be developed based on the proposed layout subject to this application.

DESIGN INTENT - FROM STATION APPROACH



Figure 10 - Draft Image of Station from Station Approach (Eastern Stair)





Artist's impression of the station from Station Square - daytime view

Figure 11 - Draft CGI of view from Station Square with Phase 1 adjoining context

DESIGN INTENT - FROM STATION SQUARE



Figure 12 - Draft Image of Station from Station Square (Western Stair)





Impression of the new railway station from Meridian Way - daytime view

Figure 13 - Draft CGI of Station Building from Meridian Way/ Glover Drive junction



View of bridge crossing and station entrance, facing the proposed Phase 1 development

Figure 14 - Draft Image of Station Bridge moving west towards Phase 1

## 6.4 Transport and Highways

### *Pedestrian and Cycle Access*

- 6.4.1 The new station is planned to be operational from May 2019. However, much of the remaining Phase 1 development will be under construction at this time, not expected to be fully complete until December 2023. The phasing plan submitted to discharge condition 4 of the OPP (see application reference 17/02954/CND) demonstrates how safe and convenient access to the station building from the west will be possible during the construction period from Kimberley Road and Albany Road (see Figure 14). Immediately adjoining areas of public realm including the station loop road directly to the south will be available from station opening.

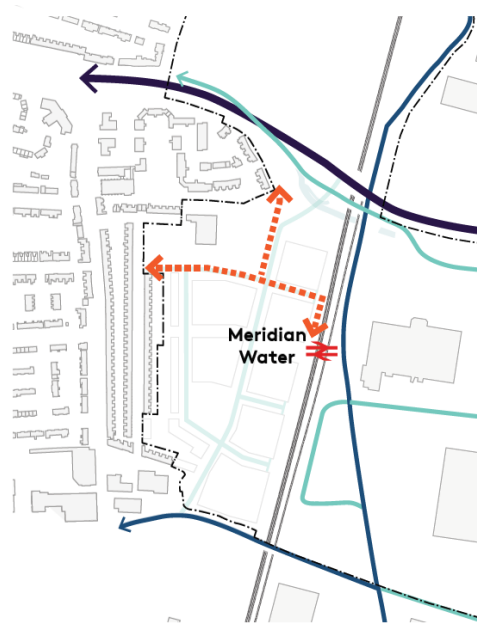


Figure 14 - Indicative Pedestrian and Cycle Access routes during construction

- 6.4.2 The public realm on the eastern side of the station will facilitate pedestrian and cycle access from station opening. Improvements to the Meridian Way/ Glover Drive junction were secured as part of the OPP to include the provision of a new signalised pedestrian crossing to improve access to and from the station. Condition 80 of the OPP requires that this junction be operational no later than 3 months after station opening.
- 6.4.3 Cycle parking is to be provided in the public realm on the east and west sides of the station. These spaces fall outside of the Station Building Site but are required to be provided at the point of station opening as per condition of the OPP. Full details of the type and number of spaces will be provided as part of Station Public Realm Site details.

- 6.4.4 The station loop road sits to the south of Station Approach to the west of the Tear Drop Site and Meridian Way which will provide vehicular access to the station for authorised vehicles only. Full management details for this road are required to be submitted as part of the Station Public Realm Site details and will form part of the wider masterplan strategy.

## **7. Conclusion**

- 7.1 The proposed layout of the Station Building Site is in accordance with the parameters of the OPP for the Phase 1 site. It would relate well to the surrounding routes and spaces and provide suitable access to the station as well as forming the basis for a high-quality building of sufficient prominence in the surrounding area. Approval of the layout is therefore recommended subject to the conditions listed in section 9 of this report.
- 7.2 Officers are satisfied that the determination of the layout would not jeopardise the future design quality or detailing that could be achieved for the superstructure on the basis of the draft images presented above. Agreement is therefore sought for the determination of the remaining reserved matters relating to the Station Building Site being Scale, Appearance and Landscaping under delegated authority.

## **8. Recommendation**

- (i) Approve reserved matters required by condition 5, part (i) (Layout) in respect of the Meridian Water Station Building Site only, pursuant to Outline Planning Permission 16/01197/RE3 dated 10/07/2017 subject to the conditions listed in section 9 and;
- (ii) Agree to grant the Head of Development Management delegated authority to approve subsequent reserved matters required by condition 5, parts (ii)-(iv) in relation to the Meridian Water Station Building Site pursuant to Outline Planning Permission 16/01197/RE3 dated 10/07/2017 on the basis of the illustrative details presented in this report.

## **9. Conditions**

- 1) Prior to the commencement of the superstructure full details of the seating to be provided on the bridge deck shall be submitted to and approved in writing by the Local Planning Authority. The seating shall be provided prior to first opening of the station and maintained thereafter.

Reason: To safeguard the appearance of the development and ensure appropriate and inclusive seating is provided.

- 2) Prior to the commencement of the relevant part of the works, details of the interim treatment and finishes to land and structures for the passive provision of Platform 4 and associated lift shafts and stairway openings and future platform

extensions to Platforms 1-3 shall be submitted to and agreed in writing by the Local Planning Authority. The development shall be carried out in accordance with these details.

Reason: To safeguard the appearance of the development.

3) Prior to the construction of the east and west public staircases to the station bridge, full details of the cycle gutters shall be submitted to and approved in writing by the Local Planning Authority. The development shall be carried out in accordance with the approved details and maintained thereafter.

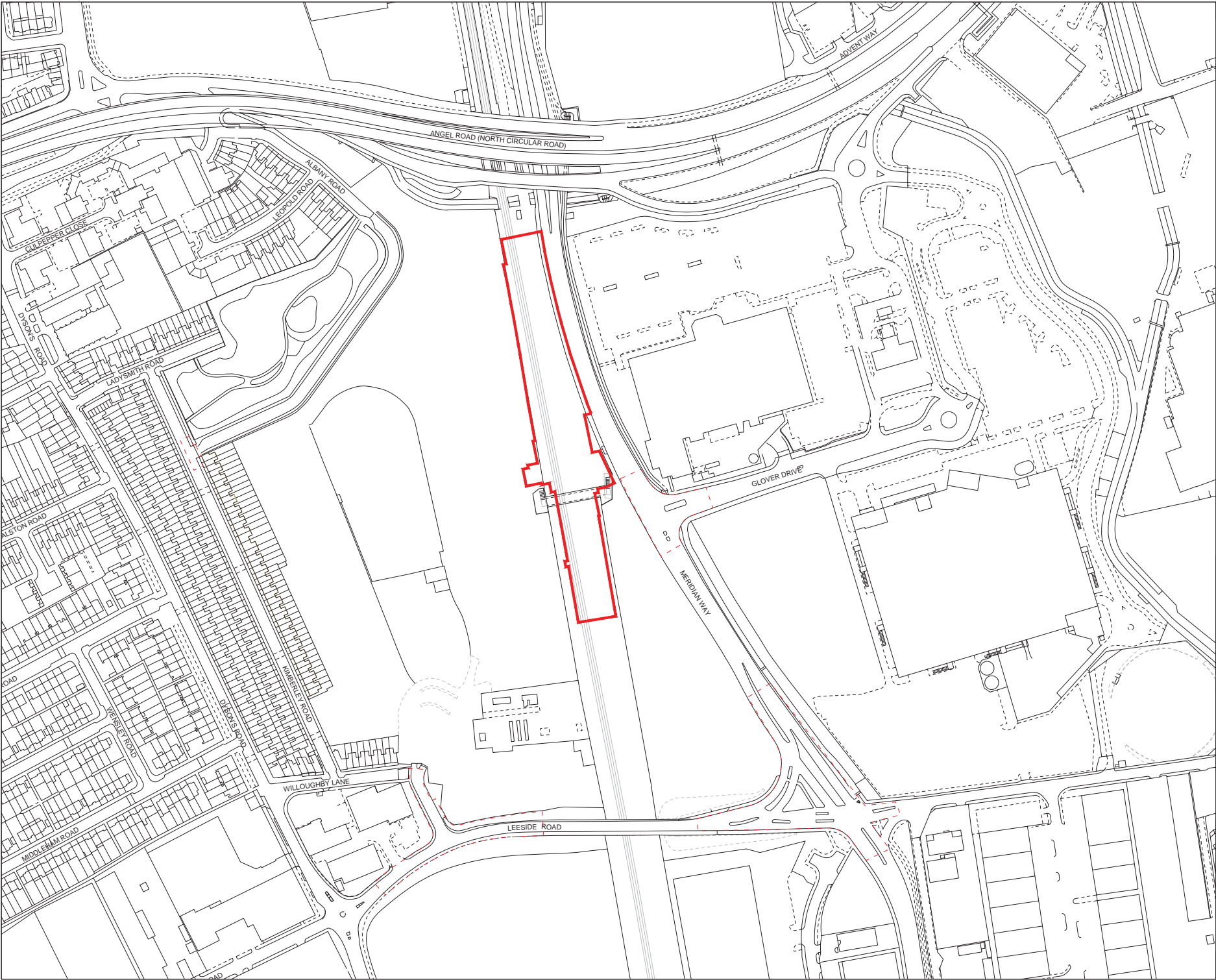
Reason: To provide suitable bicycle access over the bridge.

4) Bicycle parking shall be provided on both the east and west sides of the station prior to first operation of the station.

Reason: To secure sufficient cycle parking provision and promote sustainable travel in accordance with London Plan (2016) policy 6.9.

5) Prior to commencement of above ground works a Public Route Access Management Plan shall be submitted to and approved in writing by the Local Planning Authority. The report shall set out the arrangements for step-free access across the railway during station opening hours in the event of lift failure. The report shall include details of provision of an alert system in a public location in close proximity to the lifts so that station staff can be contacted in the event of lift failure. The approved plan shall be available to station staff at all times and access shall be managed in accordance with the approved details.

Reason: To ensure public access across the bridge is provided as far as is practicable in the event of lift failure on either or both sides of the bridge.



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REV	DATE	BY	DESCRIPTION	CHK	APP
A	26/06/2017	CGT	FIRST ISSUE		

DRAWING STATUS: FOR INFORMATION ONLY

Client: LONDON-BOROUGH-OF-ENFIELD/NETWORK-RAIL

Consultant: VOLKERFITZPATRICK-LTD.

PROJECT: MERIDIAN-WATER-DEVELOPMENT

TITLE: STATION-BUILDING-SITE-FOR-PLANNING

SCALE 8 A1:	1:1250	CHECKED:	AW	APPROVED:	AW
CAD FILE:	STATION BUILDING SITE FOR PLANNING	DESIGN ORIGIN:	CGT	DATE:	26/06/2017
PROJECT No:	C12107	DRAWING No:	MERIDIAN-WATER-PLANNING-DRG-004	REV:	A







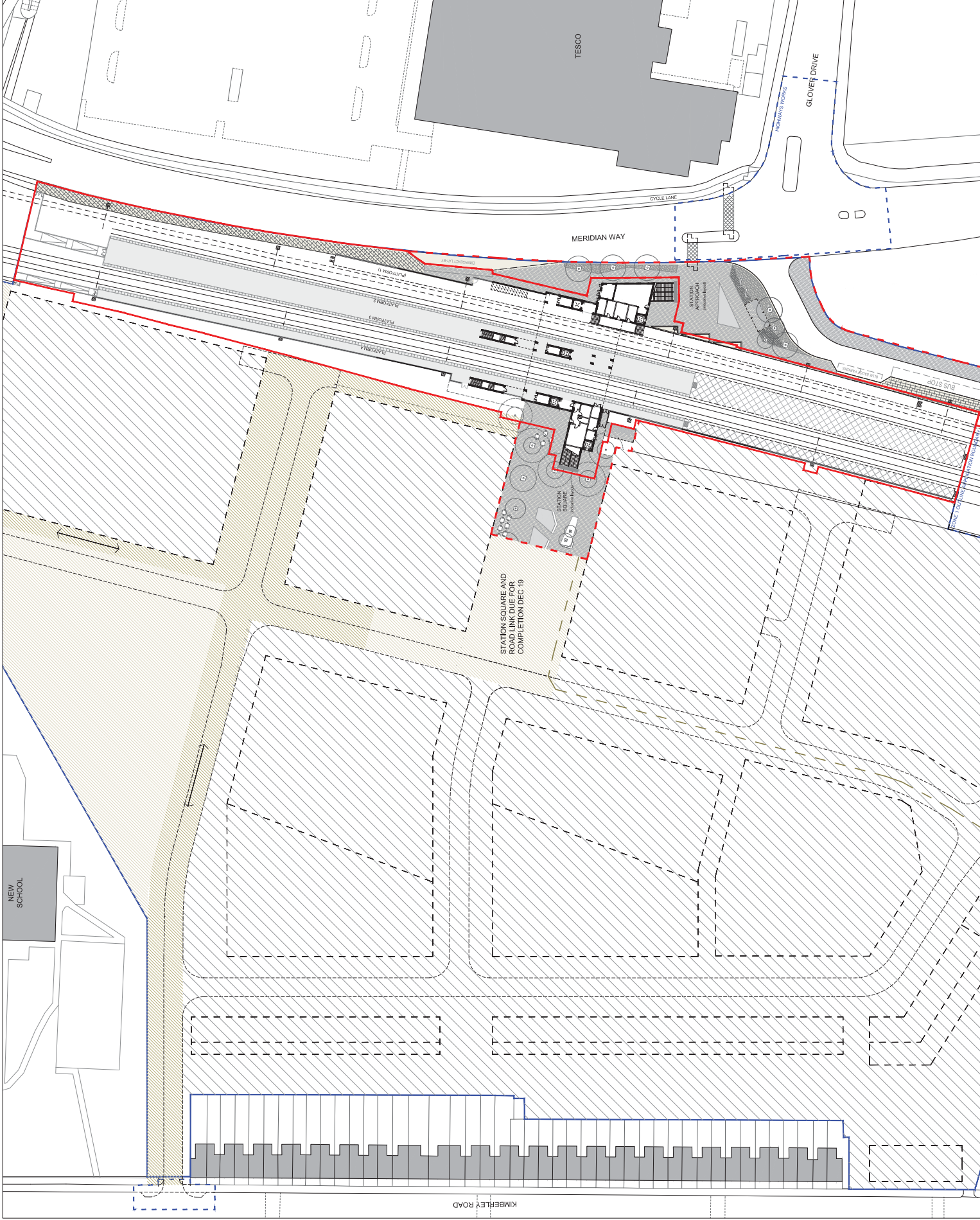
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| 01  | Red line boundary update | 30/08/2017 |
| 00  | Issued for planning      | 30/06/2017 |
| Rev | Reason for issue         | Date       |

PROJECT  
MW Angel Road Station (Meridian Water Station)  
London Borough of Enfield

# Proposed Site Plan Station Opening

DRAWING NUMBER <b>280-A-P-100-01</b>		REVISION <b>01</b>
STATUS <b>FOR PLANNING</b>		
REVISION DATE <b>30/08/2017</b>	DRAWN BY <b>ED</b>	SCALE <b>1/500 @ A1</b>
FIRST ISSUED <b>30/08/2017</b>	CHECKED BY <b>GF</b>	PROJECT NUMBER <b>280</b>







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- NOTES
- STATION BUILDING SITE
  - APPLICATION BOUNDARY (to be delivered by Network Rail)
  - APPLICANT BOUNDARY (to be delivered by the applicant)
  - INDICATIVE ZONE FOR FUTURE DEVELOPMENT (to be delivered by others)

01 Red line boundary / update  
00 Issued for planning

30/06/2017  
30/06/2017

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PROJECT  
MW Angel Road Station (Meridian Water Station)  
London Borough of Enfield

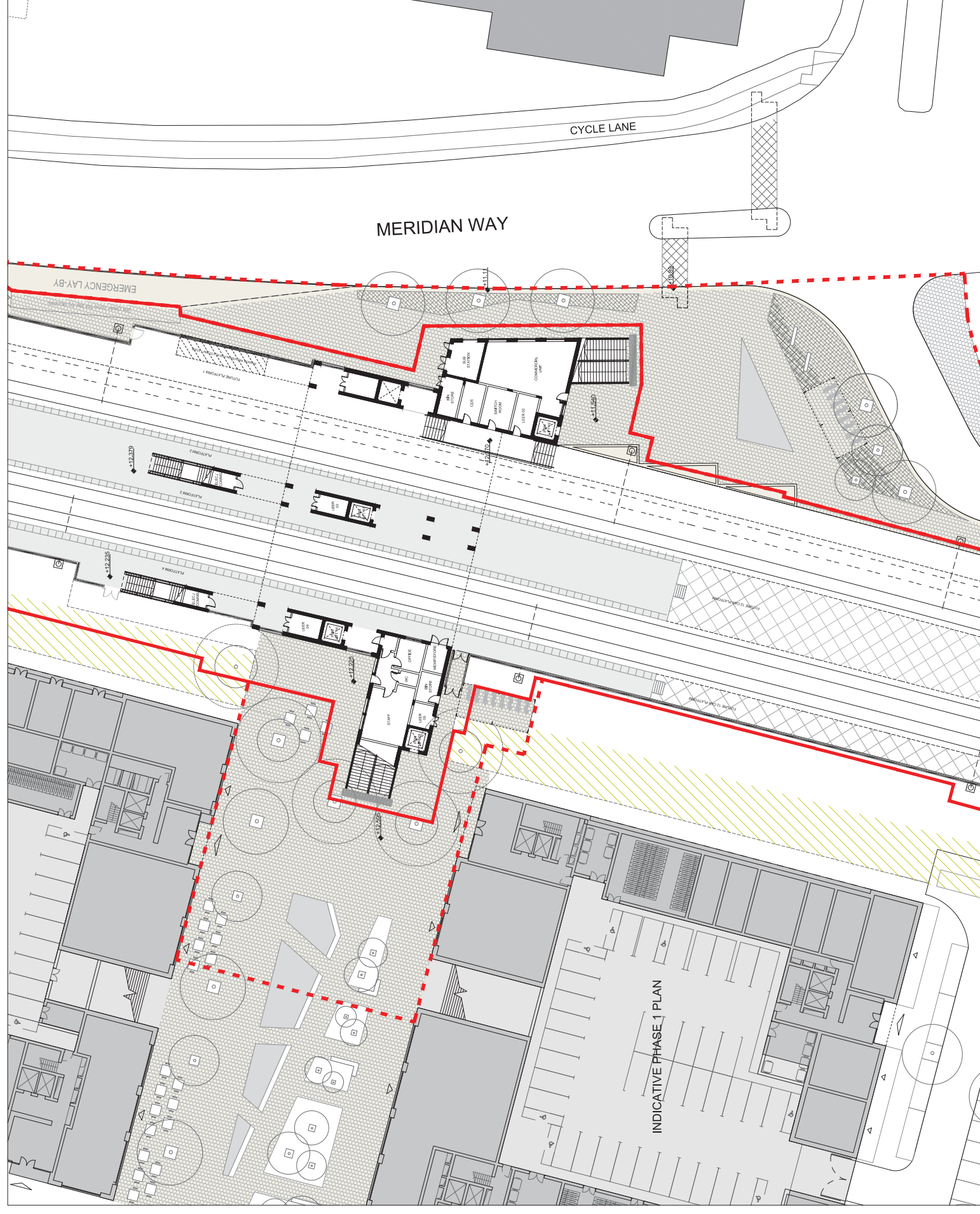
TITLE  
Proposed Ground / Platform  
Level Plan

REVISION NUMBER  
280-A-P-100-10

REVISION  
01

FOR PLANNING  
30/06/2017  
30/06/2017

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REVISIONS

Rev	Revised by	Date
0	Red line boundary / update	30/06/2017
00	Issued for planning	30/06/2017

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PROJECT  
MW Angel Road Station (Meridian Water Station)  
London Borough of Enfield

TITLE  
Proposed Bridge / Deck  
Level Plan

DOCUMENT NUMBER  
280-A-P-100-11

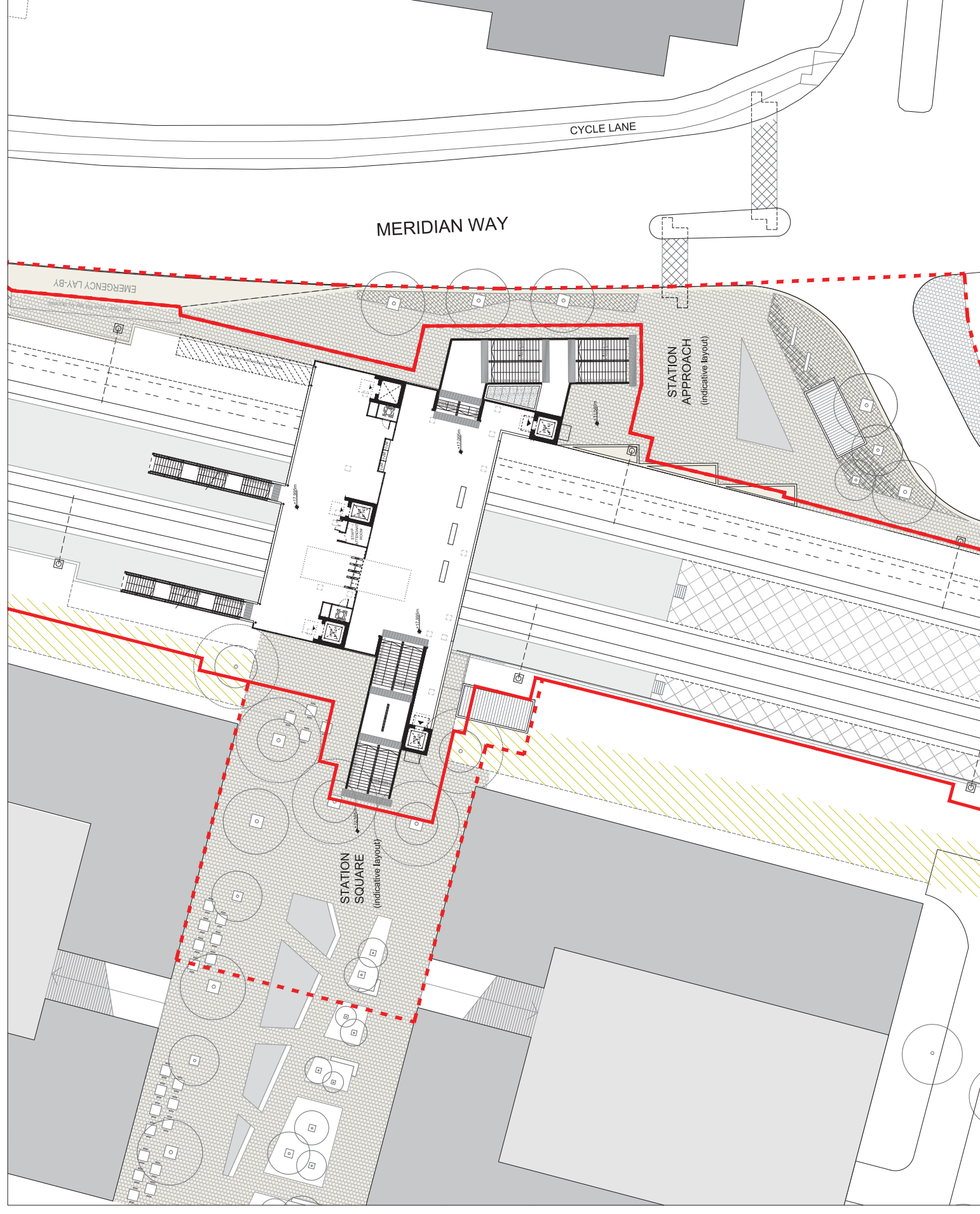
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280





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THIS DRAWING IS BASED ON DESIGNAL SURVEY INFORMATION PROVIDED BY  
OTHERS. THE ARCHITECT CANNOT ACCEPT RESPONSIBILITY FOR THE ACCURACY OF  
THIS SURVEY INFORMATION.  
ALL DIMENSIONS ARE SHOWN IN METRIC.

   	<p>ZONE 1 PLANNING APPLICATION BOUNDARY</p> <p>OFF-SITE HIGHWAY WORKS</p> <p>STATION BUILDING SITE APPLICATION BOUNDARY (to be defined by Newell &amp; Reed)</p> <p>STATION PUBLIC REALM APPLICATION BOUNDARY (to be defined by Bristol)</p>
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000	Issued for planning	30/06/2017
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NAME	PROJECTS OFFICE	DATE
<b>Karakusevic Carson Architects</b>		

2000-2001  
2001-2002  
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PROJECT

MW Angel Road Station (Meridian Water Station)

London Borough of Enfield

USE	Proposed Site Plan	Phase 1 Complete
1.00	1.00	1.00
2.00	2.00	2.00
3.00	3.00	3.00
4.00	4.00	4.00
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6.00	6.00	6.00
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DRAWING NUMBER <b>280-A-P-100-02</b>	REVISION <b>00</b>
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200 AT-100-02

AT-100

FOR ILLUSTRATIVE PURPOSES ONLY

DATE ISSUED	CHECKED BY	DRAWN BY	SCALE
30/06/2017	GF	ED	1/500 @ A1
PROJECT NUMBER			
280			

